



CITY OF COUNCIL GROVE • 205 UNION STREET • PO BOX 313  
COUNCIL GROVE, KS 66846 • 620-767-5417 • COUNCILGROVE.COM

**City Council Agenda**  
**March 18, 2025**  
**5:30 P.M. - City Hall**

WELCOME AND CALL TO ORDER

PLEDGE OF ALLEGIANCE

INVOCATION

PUBLIC COMMENT PERIOD:

Items not already on the agenda may be brought before the Governing Body. People must sign in to be eligible. (**Three-minute maximum time limit**). After three minutes, items will then be voted on to see whether to place the item on the next agenda.

**CONSENT AGENDA:**

- Minutes from the Previous Meeting: Pages 2 - 4
- Appropriations:

**Motion:**                      **Seconded:**                      **Action:**                      **Abstention:**                      **Este. Cost:**

**OLD BUSINESS:**

- Katy Depot Next Steps:
  - Ben Moore Studios – Ben Moore
  - Governmental Assistant Services – Brett Waggoner

**Motion:**                      **Seconded:**                      **Action:**                      **Abstention:**                      **Este. Cost:**

**NEW BUSINESS:**

- Morris County Court House Parking Pages 5 - 12

**Motion:**                      **Seconded:**                      **Action:**                      **Abstention:**                      **Este. Cost**

- Police and Fire Committee Recommendation: Appointment of Joseph Fricilone and Brett Samuels Page 13

**Motion:**                      **Seconded:**                      **Action:**                      **Abstention:**                      **Este. Cost**

- City Hall Software Discussion

**Motion:**                      **Seconded:**                      **Action:**                      **Abstention:**                      **Este. Cost**

**Governing Body Comments:**

**Adjournment:**

City Council Meeting Minutes  
March 4, 2025

**WELCOME AND CALL TO ORDER**

Mayor Debi Schwertfeger called the regular City Council Meeting to order. Council members present were Jason Booker, Mark Berner, Denise Hartman, Sean Honer, Nathan Adams and Sharon Haun also present were the City Administrator Nick Jones, City Attorney Brian Henderson, Assistant City Attorney Molly Priest. Others attending were Jan Sciacca, Karen Exon, Ryan McDonald, Pastor Billy Kryger, Marty Wright, Deidre Knight, Jesse Knight, Stan Bartley, Ron Jantz, Zoey Wadick, Taylor Adams, Nathan McDiffett, Ryan Moss, Daniel Winter, Steven Crichton, Vern Hay, Cody Catlin.

**PLEDGE OF ALLEGIANCE:**

**INVOCATION:** Pastor Christy Alexander

**PUBLIC COMMENT:**

Karen Exon addressed the Council regarding the deadline to respond to the city regarding annexation and listed items that needed to be addressed before leaseholders could decide. Exon expressed concern with the upcoming due date of March 17 and recommended the Council move the date between July and September 2025 to allow time for the details to be worked out.

**CONSENT AGENDA:**

Councilperson Sean Honer made a motion to approve the Consent Agenda as presented in the packet. Councilperson Jason Booker seconded the motion. Motion Carried 6 – 0. The consent agenda consisted of:

- February 4, 2025, Minutes
- February 4, 2025, to Current Appropriations.

**OLD BUSINESS**

- **Economic Impact Study Presentation: Micheal Scanlon**  
Micheal Scanlon with Our City Planning LLC. presented the City Lake Economic Impact Study to the City Council. Scanlon went through statutory requirements for annexation of noncontiguous land and said that under the statute the Economic Impact Study is a critical requirement to ensure that all financial and practical consequences are thoroughly analyzed. Scanlon went through the Executive summary which consisted of unification of governance, opportunity to unify governance between city and lake area, enhanced public services (septic concern), Consistent municipal services for all residents, economic sustainability – strengthen community cohesion, secure long-term economic sustainability, diverse residential community, mix of modest homes and high-value properties, equitable tax distribution and tax impacts, lease payment concern, fair distribution of tax responsibilities, long-term growth strategies, implement strategies benefiting both city and lake residents. Scanlon said that a 2018 BOTA ruling set annual lease value at \$4,500 per parcel and that there is a misalignment in lease payments and that the lease payments cover maintenance costs and do not reflect actual land values for use of the land. Scanlon went through the Pre-Annexation Rate with the Council Grove City Lake current mill rate at 141.415 and City of Council Grove 212.330 and Post Annexation Mill Rates for the Council Grove City Lake of 184.53 and City of Council Grove 185.408. Scanlon also went through the pros and cons for both the City Residents and City Lake Leaseholder.

- **Street and Parks/Selection Committee Recommendation: Downtown Streetscape**  
City Administrator Nick Jones presented the Streets and Parks/Selection Committee recommendation for the Downtown Streetscape. The Street and Parks/Selection Committee reviewed the request for qualification (RFQ) from three engineering firms: BG Consultants, Kaw Valley Engineering, and Swab Eaton. The committee completed a selection committee report scoring the firms on 7 different criteria with a 100-point max and ranked by final score total. BG Consultants rank as #1, Swab Eaton as #2, and Kaw Valley Engineering as #3. The Streets and Parks Committee/Selection Committee recommends the Council approve negotiations with the highest ranked firm BG Consultants. After discussion Councilperson Mark Berner. The motion was seconded by Councilperson Sharon Haun. Motion carried 6 – 0

## **NEW BUSINESS**

- **Riverwalk Amphitheater Contract – Backroads Concepts LLC.**  
The council discussed the potential contract with Backroads Concepts LLC. for booking and production services for Washunga Days and 3 other events. Much of the discussion focused on the 60 days before and the 60 days after clause in the contract as some on Council were worried about other events not being able to rent or use the facility. The Council also discussed the Hold Harmless clause with City Attorney Brian Henderson advising the Council that he would not recommend the agreement with the Hold Harmless Clause. The Council decided to remove the Hold Harmless Clause. Councilperson Denise Hartman made a motion to approve the agreement with the Hold Harmless Clause removed. The motion was seconded by Councilperson Jason Booker. Motion carried 6 – 0
- **Boat Dock Discussion: 124 Prairie Bluff Loop – F3**  
The Council discussed a boat dock extension for Bill Schrader 124 Prairie Bluff Loop – F3 at the City Lake. Schrader wants to extend his new dock out to 72’ off the shoreline for better water depth. After discussion and concerns expressed over the length Councilperson Mark Berner made a motion to approve the boat dock for 124 Prairie Bluff Loop with the neighbors signing off on the extension. The motion was seconded by Councilperson Sean Honer. Motion carried 5 – 1. Councilperson Nathan Adams voted against the motion.
- **CCLIP Resurfacing Project KDOT Agreement: Belfry to West City Limits**  
City Administrator Nick Jones presented the contract from KDOT for the Highway resurfacing project from Belfry to the west city limits for Summer of 2026. Administrator Jones said the grant is \$400,000 from KDOT with a match of \$240,000 that will come from the gas tax money received from the State. Councilperson Jason Booker made a motion to approve the contract with KDOT. The motion was seconded by Councilperson Denise Hartman. Motion carried 6 – 0
- **BG Consultants Design Contract: CCLIP Resurfacing Project**  
City Administrator Nick Jones presented the BG Consultants Design Contract for the CCLIP Resurfacing Project, Belfry to the west city limits. Administrator Jones said the contract for the design cost \$20,000, which will be paid out of the grant and gas tax money. After discussion Councilperson Mark Berner made a motion to approve the design contract. The motion was seconded by Councilperson Sharon Haun. Motion carried 6 – 0

• **GOVERNING BODY COMMENTS**

- **Mayor Debi Schwerdtfeger** – Set a work study session for March 26<sup>th</sup> at 5:30pm to discuss the City Lake and Executive Session.
- **Councilperson Denise Hartman** – Wished Jesse and Deidra Knight good luck.
- **Councilperson Sean Honer** – N/A
- **Councilperson Nathan Adams** – N/A
- **Councilperson Sharon Haun** – Looking forward to the upcoming events.
- **Councilperson Jason Booker** – Thanks the Street Department for fixing the potholes.
- **City Attorney Brian Henderson** – Absent
- **Assistant City Attorney Molly Priest** – N/A
- **City Administrator Nick Jones** – said he will be in Topeka on March 12<sup>th</sup> for a award ceremony for the Katy Depot CDBG grant the city received.

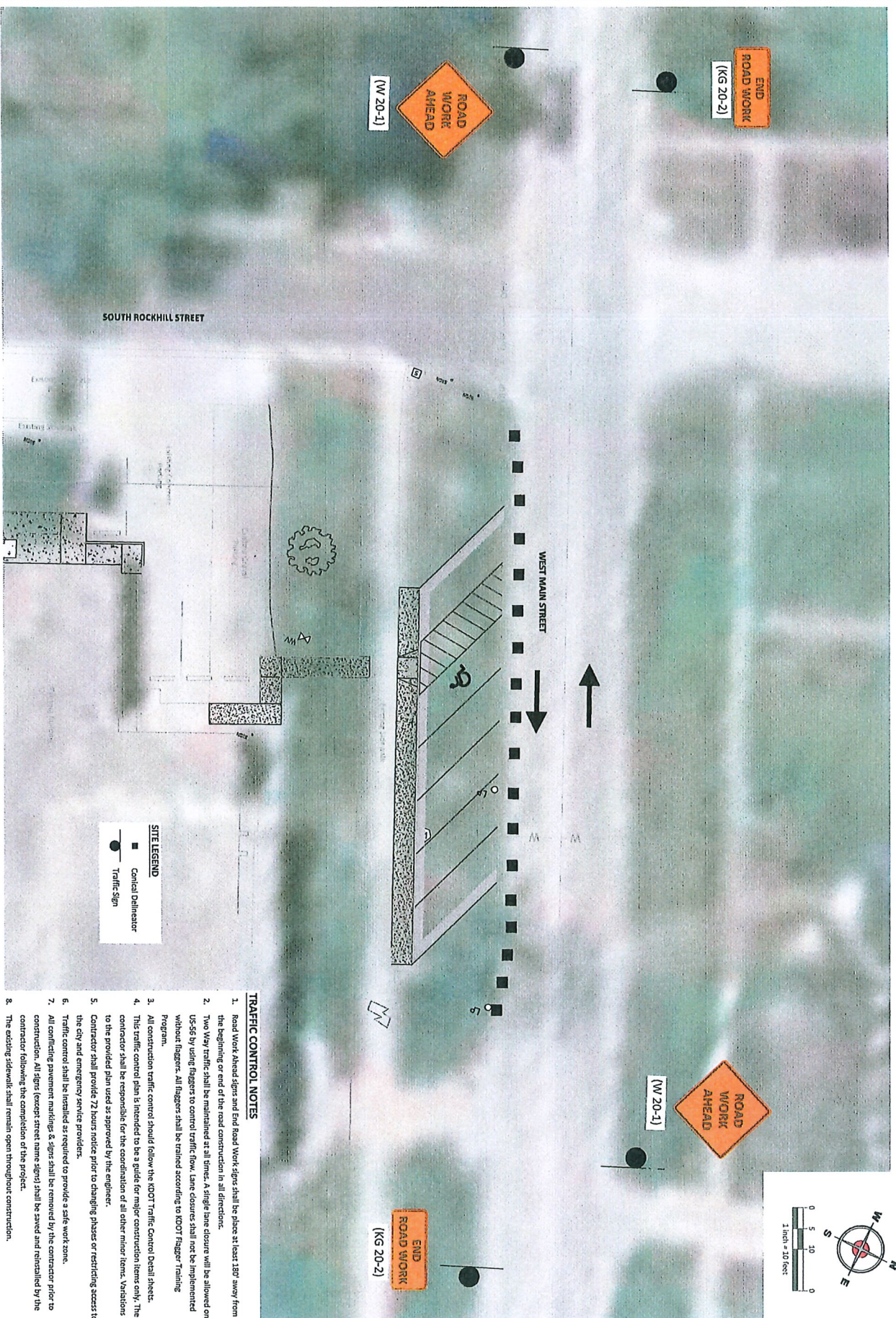
Mayor Schwerdtfeger asked for a motion to adjourn. Councilperson Sean Honer made a motion to adjourn. Councilperson Nathan Adams seconded the motion.  
 Motion carried 6 – 0

\_\_\_\_\_  
 Debi Schwerdtfeger                      Mayor

ATTEST:

\_\_\_\_\_  
 Nick Jones                      City Administrator





- TRAFFIC CONTROL NOTES**
1. Road Work Ahead signs and End Road Work signs shall be placed at least 180' away from the beginning or end of the road construction in all directions.
  2. Two Way traffic shall be maintained at all times. A single lane closure will be allowed on US-56 by using flaggers to control traffic flow. Lane closures shall not be implemented without flaggers. All flaggers shall be trained according to KDOT Flagger Training Program.
  3. All construction traffic control should follow the KDOT Traffic Control Detail sheets.
  4. This traffic control plan is intended to be a guide for major construction items only. The contractor shall be responsible for the coordination of all other minor items, violations to the provided plan used or approved by the engineer.
  5. Contractor shall provide 72 hours notice prior to changing phases or restricting access to the city and emergency service providers.
  6. Traffic control shall be installed as required to provide a safe work zone.
  7. All conflicting pavement markings & signs shall be removed by the contractor prior to construction. All signs (except street name signs) shall be saved and reinstalled by the contractor following the completion of the project.
  8. The existing sidewalk shall remain open throughout construction.

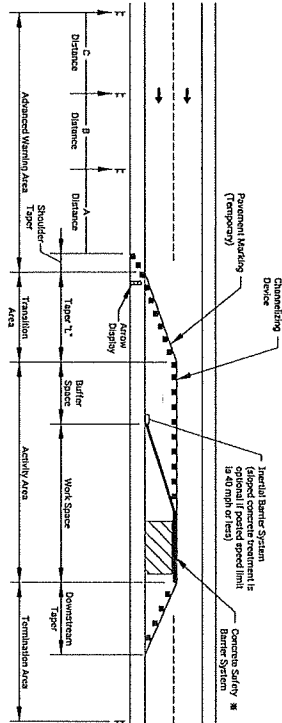
Project No: E2021-56  
 Date: 10/29/2024  
 Sheet No: S1 OF S6

**DRIGGS DESIGN GROUP, PA**  
 Surveying Engineering Planning  
 MANHATTAN - HAYS - EMPORIA - TOPEKA - GOODLAND

CITY OF COUNCIL GROVE, KANSAS  
 MORRIS COUNTY JAIL - ADDITIONAL PARKING  
 TRAFFIC CONTROL PLAN


- 1) Design Speed: Those items delegated to temporary traffic control should be designed and installed using the posted/legal speed of the roadway prior to work starting.
- 2) Minimum Lane Width: Lane width shall be a minimum of 11' (measured between centerlines of pavement markings) or as shown on the plans, or as directed by the engineer. A lane width less than 11' may require restricted roadway width signing.
- 3) Consideration should be made to separate pedestrians and, if needed, bicycle movements from both work site activity and vehicular traffic. Unless a roadway is closed to traffic, pedestrian and bicycle movements should be provided. Pedestrians should be appropriately directed with advance signing that encourages them to cross to the opposite side of the roadway. In urban and suburban areas with high vehicular traffic volumes, these signs should be placed at intersections (rather than midblock locations) so that pedestrians are not confronted with midblock work sites that will induce them to attempt skirting the work site or making a midblock crossing.
- 4) When existing pedestrian facilities are disrupted, closed, or relocated, the temporary facilities shall be detectable and include accessibility features consistent with the features present in the existing pedestrian facility.
- 5) When the driving surface open to traffic is milled or is a temporary surface made of loose material, or when directed by the engineer a W8-15 (Grooved Pavement) or W8-7 (Loose Gravel) sign shall be used in advance of the work site. Signs A, W8-15 and W8-7 signs shall be used to supplement the W8-15 or W8-7 signs. All signs shall be displayed as long as the condition is present.
- 6) Alternative temporary rumble strip options may be available. Please contact the Temporary Traffic Control Unit for more information at 785-296-1179 or 785-296-1183.

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**TYPICAL WORK ZONE COMPONENTS**

\* When concrete barrier system is used, portable channelizing devices are not needed along the longest barrier section.

Minimum advance warning sign spacing (in feet):

SPEED (MPH) *	A	B	C
URBAN (40 MPH OR LOWER)	700	700	100
URBAN (45 MPH OR HIGHER)	350	350	350
RURAL (55 MPH OR LOWER)	500	500	500
RURAL (60 MPH OR HIGHER)	750	750	750
EXPRESSWAY/FREEWAY	1000	1500	2500

\* Posted speed prior to work starting  
 The minimum spacing between signs shall be no less than 100', unless directed by the engineer.  
 The spacing between any signs may be increased beyond the minimum values in the table above upon approval by the engineer in order to maximize visibility.

Buffer Space

SPEED (MPH) *	20	25	30	35	40	45	50	55	60	65	70	75
LENGTH (ft)	115	155	200	250	305	360	425	495	570	645	730	820

\* Posted speed prior to work starting  
 Neither work activity nor storage of equipment, vehicles, or material should occur in the buffer space. When a protection vehicle is placed in advance of the work space, only the space upstream of the vehicle constitutes the buffer space.  
 If temporary concrete safety barrier system is used to separate approaching traffic from the work space, the barrier system shall be considered part of the activity area. A full lane width should be available throughout the length of the buffer space. See typical work zone components above.

**Taper Formulas:**

$L = WS$  for speeds of 45 MPH or more  
 $L = WS^2/60$  for speeds of 40 MPH or less  
 Where:  $L$  = Minimum length of taper in feet  
 $S$  = Numerical value of posted speed  
 $W$  = Width in offset feet  
 Shifting Taper =  $1/2L$   
 Shoulder Taper =  $1/3L$

- Channelizer Placement:**
- (1) The spacing between devices in transition area (taper) should not exceed a distance in feet equal to 1/2 the posted speed limit in mph prior to work starting.
  - (2) The spacing between devices in the advanced warning area and the activity area should not exceed a distance in feet equal to two times the posted speed limit in mph prior to work starting.
  - (3) Channelizing devices shall be placed for optimum visibility, normally at right angles to the traffic flow.
  - (4) Place directional indicator barricades in series to direct traffic onto the new path. The arrow sign should not be visible to opposing traffic.
  - (5) Alternating diagonal orange and white striping must slope downward in the direction traffic is expected to pass.

NO.	REVISION	DATE	BY	CHKD.
1	ISSUED			
2	REVISED			

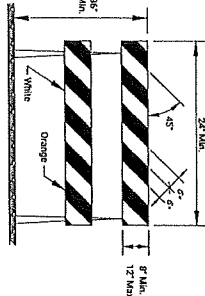
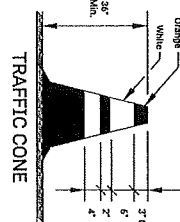
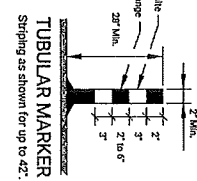
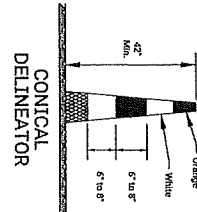
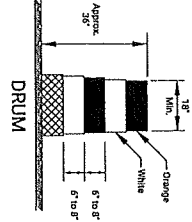
TRAFFIC CONTROL GENERAL NOTES

Project No.: E2021-56  
 Date: 10/29/2024  
 Sheet No.: 52 OF 56

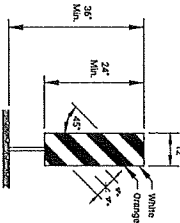
**DRIGGS DESIGN GROUP, PA**  
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CITY OF COUNCIL GROVE, KANSAS  
 MORRIS COUNTY JAIL - ADDITIONAL PARKING  
 TRAFFIC CONTROL DETAILS

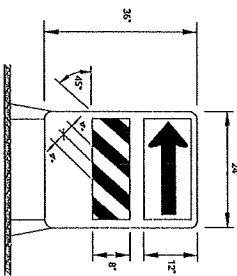
NO.	REVISION	DATE



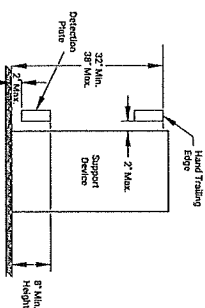
**TYPE 2 BARRICADE**  
For calls less than 36' long, 4' wide stripes may be used. All stripes shall slope downward to the traffic side for channelization.



**VERTICAL PANEL**  
The stripes shall slope downward to the traffic side for channelization.



**DIRECTION INDICATOR BARRICADE**  
The stripes shall slope downward in the direction traffic is to pass. The direction indicator barricade shall be used in series to direct the motorist into the intended lane of travel.



**PEDESTRIAN CHANNELIZER**  
1. Support device shall not project beyond the detection plate into the pathway.  
2. Hand trailing edges and detection plates are optional for continuous walls.  
3. Interconnect pedestrian channelizers to prevent displacement and to provide continuous guidance through or around work areas.  
4. Interconnect pedestrian channelizers to prevent displacement and to provide continuous guidance through or around work areas.  
5. Treat height differentials > 1/2\"/>

Item	Location	Application											
		Cross-overs	Shoofly Diversions	Tangents	Tapers	Ramps	Head to Head	Object Identifier	Lead-in Devices	Cores			
Portable	Drums	Yes	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes			
	Conical Delineators	Yes	Yes	Yes	Yes	(1)	Yes	Yes	Yes				
	Vertical Panels	(2)	(2)	(2)	(2)	(2)	(1,2)	Yes	(2)	(2)			
	Direction Indicator Barricade	No	No	No	No	No	No	No	No	No			
Fixed	Type 2 Barricade	(2)	(2)	(2)	(2)	(2)	No	Yes	(4)	(4)			
	Traffic Cones	No	No	(4)	(4)	(4)	No	(4)	(4)	(4)			
	Tubular Markers	(3)	(3)	(3)	(3)	(3)	Yes	No	Yes	(2,3)			
	Vertical Panels	(3)	(3)	(3)	(3)	(3)	Yes	Yes	(2,3)	(2)			

- (1) Not allowed on conical delineator along freeway or expressways.
- (2) The stripes shall slope downward to the traffic side for channelization.
- (3) May be used upon the approval of the engineer.
- (4) Daytime operations only.

DATE	PROJECT NO.	VOL.	SHEET NO.	TOTAL SHEETS
XXXX	XX-XXXX-XX	XXXX	XXX	XXX

TRAFFIC CONTROL CHANNELIZING DEVICES

SCALE: \_\_\_\_\_

DATE: \_\_\_\_\_

PROJECT: \_\_\_\_\_

DESIGNED BY: \_\_\_\_\_

CHECKED BY: \_\_\_\_\_

APPROVED BY: \_\_\_\_\_

Project No.: E2021-56  
 Date: 10/29/2024  
 Sheet No.: 56 of 56

**DRIGGS DESIGN GROUP, PA**  
 Surveying Engineering Planning  
 MANHATTAN - HAYS - EMPORIA - TOPEKA - GOODLAND

CITY OF COUNCIL GROVE, KANSAS  
 MORRIS COUNTY JAIL - ADDITIONAL PARKING  
 TRAFFIC CONTROL DETAILS

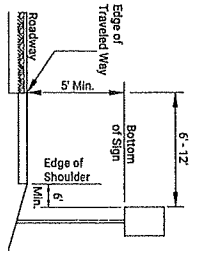
#	Revised	Date



**SIGN LAYOUT INFORMATION**

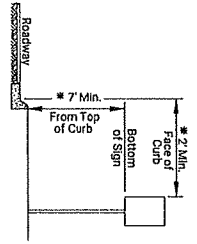
<b>END ROAD WORK</b> KG20-2 Std. Size Expwy/Freeway 6' C 48" x 24"	<b>GROOVED PAVEMENT</b> WB-15 Std. Size Expwy/Freeway 8' D 48" x 48"
<b>WAIT FOR PILOT CAR</b> KG20-5 Std. Size Expwy/Freeway 6' C 48" x 24"	<b>LOOSE GRAVEL</b> WB-7 Std. Size Expwy/Freeway 8' D 48" x 48"
<b>ROAD ZONE</b> KM-20 Std. Size Expwy/Freeway 3' C 24" x 6" 48" x 12"	<b>UNEVEN LANES</b> WB-11 Std. Size Expwy/Freeway 8' D 48" x 48"
<b>NEXT X MILES</b> W7-3a Mileage to be Determined by the Engineer.	<b>SHOULDER DROP-OFF</b> WB-17P (Optional) Std. Size Expwy/Freeway 30" x 24"
<b>NB US-75 CLOSED FOLLOW DETOUR</b> SP-Q1 (Special Sign)	<b>US-75 CLOSED NORTH OF Topeka FOLLOW DETOUR</b> SP-Q2 (Special Sign)

All city names and street names on special signs and destination signs must have upper and lower case letters.



**RURAL**

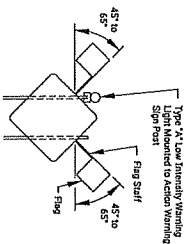
- 1) Ground-mounted signs shall be mounted at a minimum height of 5' measured from the bottom of sign to the near edge of the pavement.
- 2) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.
- 3) The height of the secondary sign may be measured from the bottom of the sign to the near edge of the pavement. Signs shall not overlap each other.



**URBAN**

- 1) Signs shall be mounted at a minimum height of 7' measured from the bottom of sign to the near edge of the pavement.
- 2) Neither portable nor permanent sign supports should be located on sidewalks or areas designated for pedestrian or bicycle traffic.
- 3) Signs mounted lower than 7' should not project more than 4" into pedestrian facilities.
- 4) The height from of the secondary sign mounted below another sign may be 6' measured from the bottom of sign to the near edge of the pavement. Signs shall not overlap each other.
- 5) Large signs having an area exceeding 50 square feet installed on multiple breakaway posts shall be mounted a minimum of 7' above the ground.

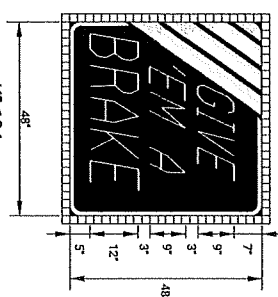
- 6) Pedestrian detour signing shall be a minimum of 2' measured from the top of the sign and shall not encroach into the walkway nor shall it project beyond the back of curb.



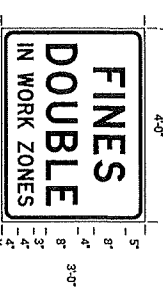
When the sign width is equal to or greater than 5', three or more wood posts may be used with a minimum of 4' between the centerline of each post. All signs less than 5' in width shall use a maximum of two wood posts.

In the case of hinged rock when driving posts:

1. Shift the sign location. Do not violate minimum sign spacing.
2. With the engineer's approval, use acceptable alternative sign stands.



KI-104a



KI-105a

DATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
XXXX-XX-XXXX-XXXX	XXXX	XXX	XXX	XXX

Sign Number	SIZE	SHAPE
Give A Few A Brake	4'-0" x 4'-0"	Rectangular
Letter Height	1.0"	
Border Width	0.7"	
Corner Radius	3.0"	
Signs Width	3.0"	
Mounting	Ground	
Background	Color: Black	
Legend/Border	Type: Reflective	
Color:	White	
Legend Font	Dutch BI Roman SWC	
Stripes	Type: Reflective	
Color:	Orange	

Sign Number	FINES DOUBLE
Width x Height	4'-0" x 3'-0"
Border Width	0.7"
Corner Radius	3.0"
Mounting	Ground
Background	Type: Reflective
Color:	White
Legend/Border	Type: Non-reflective
Color:	Black

Dimensions in inches

Font	LETTER SPACINGS	Spacing	Letter
20.0	F I N E S	3.0	8.0
17.0	D O U B L E	3.0	8.0
15.0	I N W O R K Z O N E S	3.0	4.0
13.0	I N W O R K Z O N E S	3.0	4.0
11.0	I N W O R K Z O N E S	3.0	4.0
9.0	I N W O R K Z O N E S	3.0	4.0
7.0	I N W O R K Z O N E S	3.0	4.0
5.0	I N W O R K Z O N E S	3.0	4.0
3.0	I N W O R K Z O N E S	3.0	4.0

Notes:

- Typically, there are two sets of informational signs installed per project one for each direction of traffic.
- Install signs a minimum of 500' in advance of the road work ahead sign. The engineer may designate a more appropriate location if conditions dictate.
- The informational signs are not to interfere with the traffic control signs for the project.

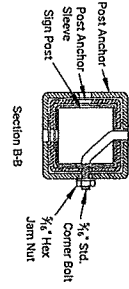
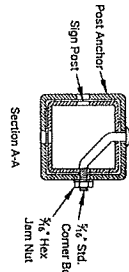
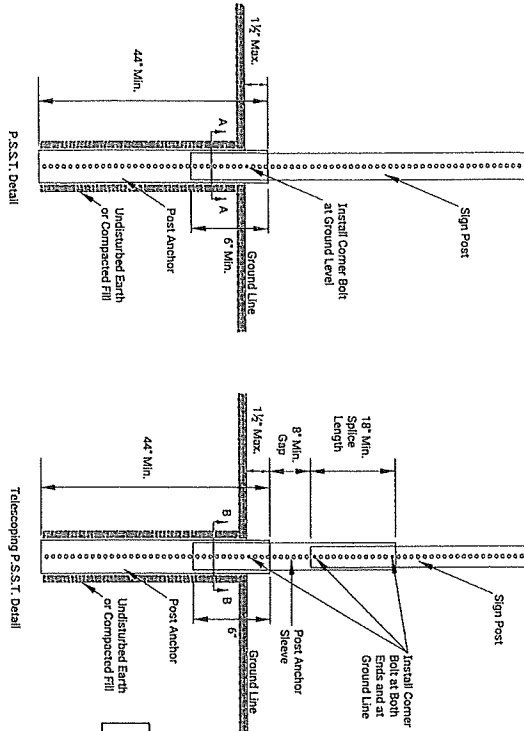
TRAFFIC CONTROL SIGN INFORMATION

NO.	DATE	REVISION	BY	CHKD.
1				
2				
3				

TE710

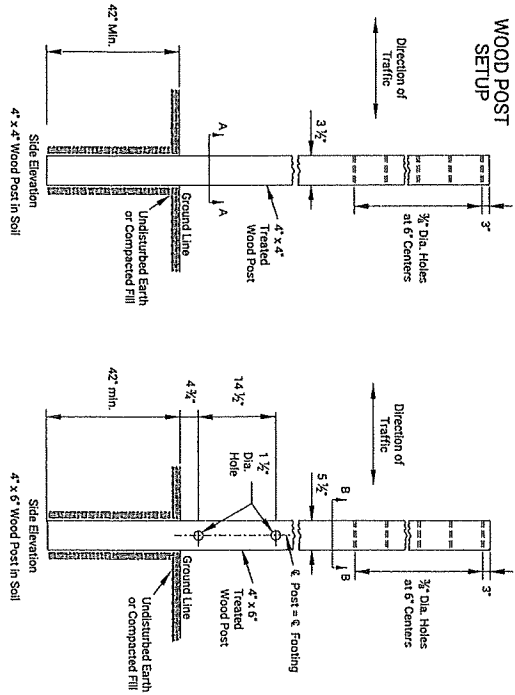
NO.	DATE	REVISION	BY	CHKD.

PERFORATED SQUARE STEEL TUBE (P.S.S.T.) POST SETUP



Details for 2 1/2" or 2 3/4" sign posts  
Place bolts in the same corner along each sign post.

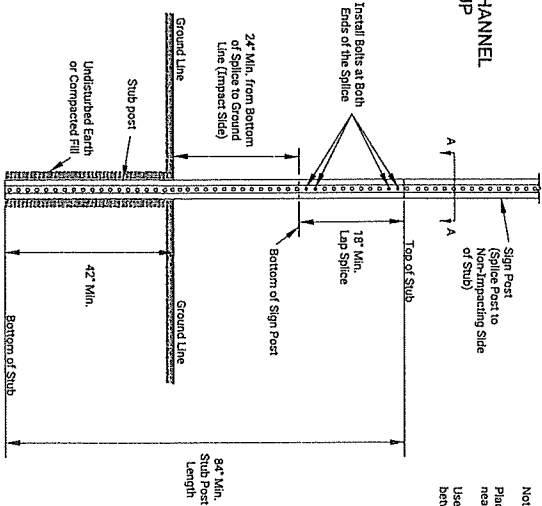
WOOD POST SETUP



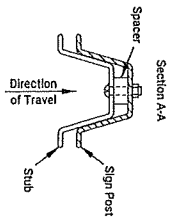
DATE	REVISION	Y&R	SHEET NO.	TOTAL SHEETS
XXXX	XX-XXXX-XX	XXXX	XXX	XXX

See TE710 for Additional Details and Requirements

3 LB/F U-CHANNEL SETUP



Notes:  
Place two bolts at both ends of the splice through the holes nearest the ends of the splice.  
Use manufacturer recommended spacers over the bolts between the spliced pieces of U-Channel.



NO.	DATE	DESCRIPTION	BY	CHECKED
1				
2				

TE712

KANSAS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL  
SIGN POSTS

Project No.: E2021-56  
Date Plotted: 10/29/2024  
Sheet No.: 55 OF 56



**DRIGGS DESIGN GROUP, PA**  
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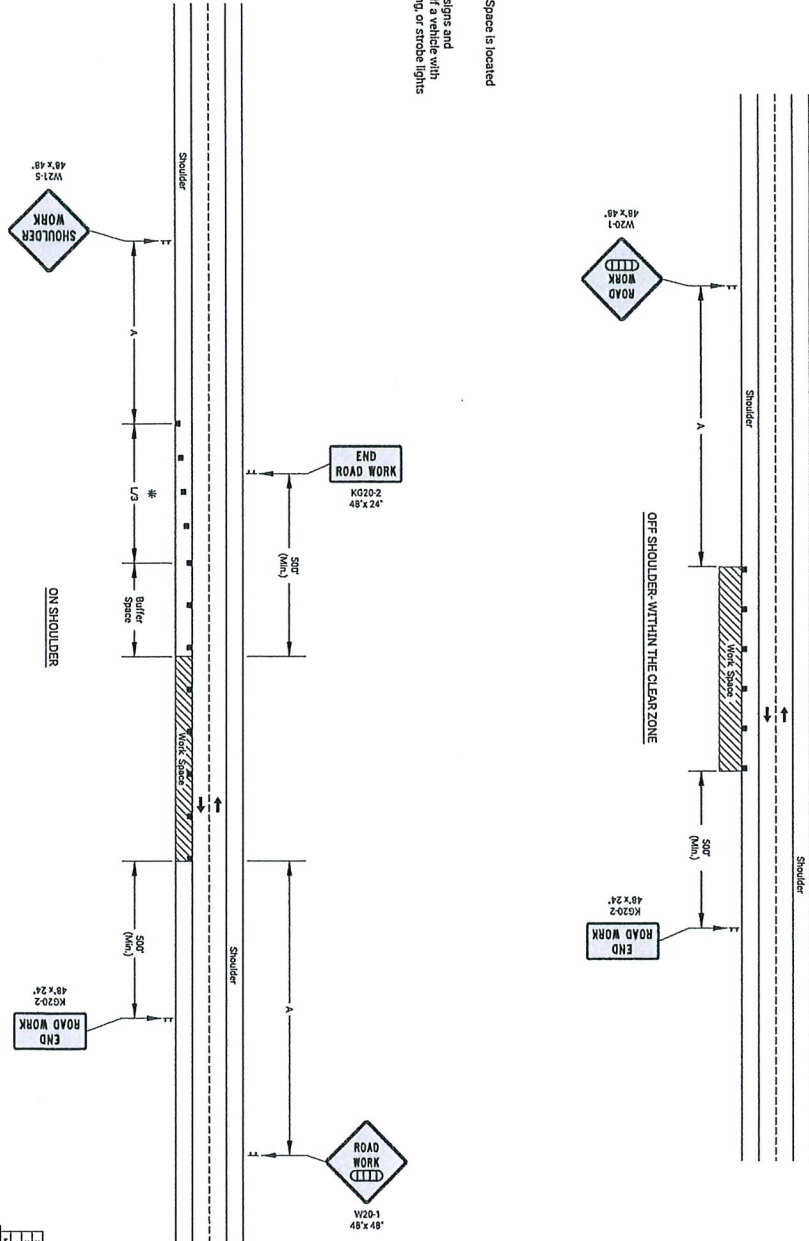
CITY OF COUNCIL GROVE, KANSAS  
MORRIS COUNTY JAIL - ADDITIONAL PARKING  
TRAFFIC CONTROL DETAILS

NO.	DESCRIPTION	DATE

Notes:  
 No traffic control is required if the Work Space is located outside of the clear zone.  
 For operations of 60 minutes or less, all signs and channelizing devices may be eliminated if a vehicle with high-intensity rotating, flashing, oscillating, or strobe lights is used.

Channelizing Device  
 CDM Ahead, 1500 ft, or 1 Mile

\* Omit taper if paved shoulder is less than 8' wide.



STATE	PROJECT NO.	YEAR	SHEET NO.	TOTAL SHEETS
KANSAS	202-02-33-000-03X	2024	20X	22X

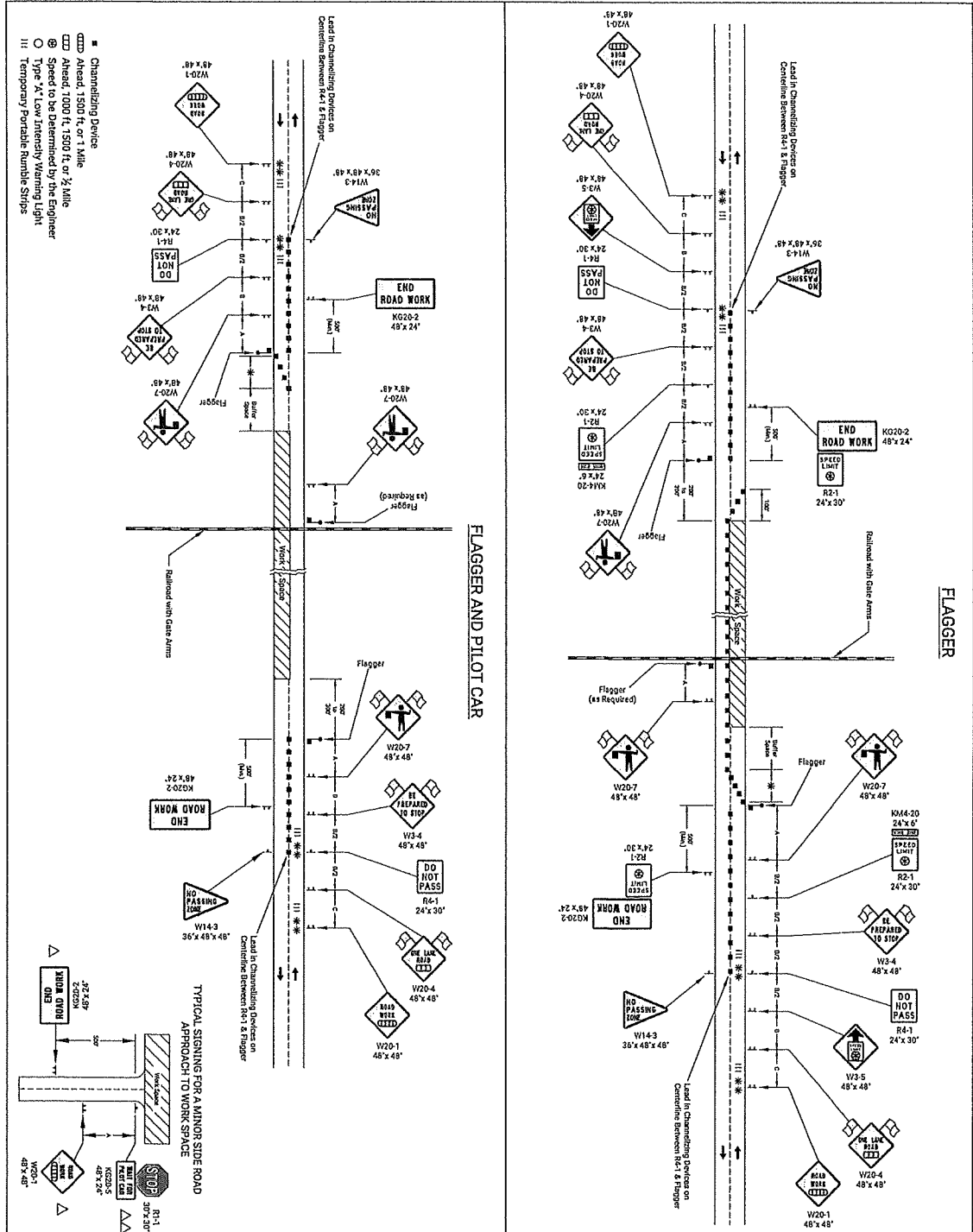
PROJECT NO.	202-02-33-000-03X
DATE	10/29/2024
PROJECT NAME	TRAFFIC CONTROL SHOULDER WORK UNDIVIDED ROADWAY
PROJECT LOCATION	TE/20
PROJECT TYPE	TRAFFIC CONTROL
PROJECT STATUS	PLANNING
PROJECT PHASE	PLANNING
PROJECT DESCRIPTION	TRAFFIC CONTROL SHOULDER WORK UNDIVIDED ROADWAY
PROJECT NUMBER	202-02-33-000-03X
PROJECT SHEET	20X
PROJECT TOTAL SHEETS	22X

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CITY OF COUNCIL GROVE, KANSAS  
 MORRIS COUNTY JAIL - ADDITIONAL PARKING  
 TRAFFIC CONTROL DETAILS

No.	Revision	Date

Project No.: E2021-56  
 Date: 10/29/2024  
 Sheet No.: 56 OF 56



**USE TEXT1 FOR FLAGGER OR PILOT CAR ON ROADWAYS WITH CONCRETE SHOULDERS GREATER THAN 8 FT.**

Notes:  
 \* Trucks hauling material to the project should STOP at the flagger. After stopping, upon approval of the Engineer, trucks may be allowed to move around the flagger.  
 \* Place a flagger at all highway and major collector intersections and at grade railroad intersections with lights and gates in the work space to control traffic crossing the tracks to the left of the gate arm. The need for a flagger at minor side road intersections shall be determined by the Engineer. Place a W20-7 (flagger) sign on each side road that is controlled by a flagger.  
 \* Existing signs shall not be covered or removed between flagger stations.  
 \* Temporary rumble strips may be used in lieu of lead in channelizing devices when the roadway is less than or equal to 30' including paved shoulders. When extending circumstances exist, the Area Engineer may elect to install rumble strips on the lead in channelizers and the rumble strips.  
 \* Minimum six (6) channelizers spaced at 20' intervals.  
 \* \* Optional rumble strips may be placed. One set between the W20-1 and W20-4, and one set between the Rd-1 and W3-4, on each approach.  
 \* Not required on substantial maintenance projects (1R).  
 \* The K620-5 (WAIT FOR PILOT CAR) sign shall be attached to the existing stop sign post.  
 \* The K620-5 sign shall be placed immediately in front of the existing stop sign, a minimum of 6' below the bottom of the stop sign. The sign should be removed or covered when there is no pilot car.  
 \* TYPICAL SIGNING FOR HIGHWAY OR MAJOR COLLECTOR APPROACH TO WORK SPACE

PROJECT NO.	E2021-56
DATE	10/29/2024
SHEET NO.	57 OF 57

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**CITY OF COUNCIL GROVE, KANSAS**  
**MORRIS COUNTY JAIL - ADDITIONAL PARKING**  
**TRAFFIC CONTROL DETAILS**



CITY OF COUNCIL GROVE • 205 UNION STREET • PO BOX 313  
COUNCIL GROVE, KS 66846 • 620-767-5417 • COUNCILGROVE.COM

### Police and Fire Committee Recommendation

The Police and Fire Committee recommend the Council approve the appointment of Joseph Fricilone and Brett Samuels to the Council Grove Fire Department as Volunteer Fire Fighters. Appointment is contingent upon passing drug and alcohol screening, physical, and background checks.